



## **355 Franktown Road, Town of Carleton Place**

Design Brief  
January 31, 2024



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# 1.0 Introduction

## 1.1 Application Submission

The provided Design Brief is submitted in support of the associated Development Applications, noted below, under review for the subject property, 355 Franktown Road, Carleton Place, Ontario, the “subject property”. The legal description of the subject property is, **Part 1, Plan 27R-3135, South West Half Lot 15, Concession 11, Township of Beckwith**. The Design brief was undertaken in accordance with Section 6.13 of the Town of Carleton Place Official Plan and the provided Terms of Reference.

### Subdivision Application

The Subdivision application is required to subdivide the subject property into three blocks, Block 1 – Proposed Apartment dwellings (2 buildings and 96 residential dwellings), Block 2 – Proposed Townhome dwellings (1 townhome building and 6 townhome dwellings), and Block 3 – Existing Commercial site, to accommodate the development as proposed.

### Class 3 Development Permit

A Class 3 Development Permit, which includes requested variances, is required to permit the proposed development as conceived.

The following variances are proposed:

- / Increase Exterior Side Yard Build-Within Area (Townhouse Dwellings) to 8.5 metres from between 4.5 metres and 7.5 metres; and
- / Increase Front Yard Build-Within Area (Apartment Dwellings) to 10.6 metres from between 4.5 metres and 7.5 metres.

## 1.2 Proposed Development

### 1.2.1 Development Statistics

The proposed development includes six townhouse dwellings and 96 apartment dwellings. The site includes two four-storey apartment buildings, each of which features 48 of the 96 dwellings. All townhouse dwellings are included within a single six-unit building.

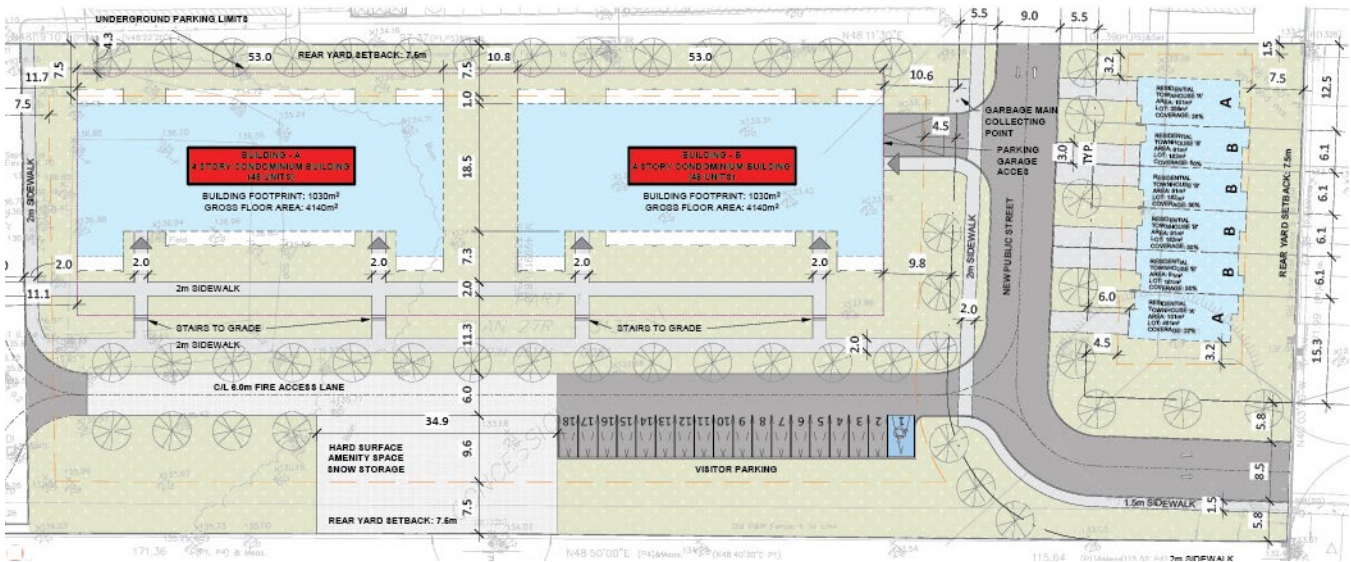


Figure 1: Propose Site Plan.

Each townhouse dwelling includes two parking stalls, one in the garage and one on the driveway in front of the garage. The apartment buildings include 130 underground parking stalls and 18 at-grade stalls, including an accessible stall.

The proposed development also includes a new public road, which will connect to future development located to the north and north-east of the subject property. Additionally, a fire access lane is also proposed at the rear of the existing mall, which will provide interim access redundancy for the development to the west, which is currently subject to a subdivision application.

### 1.2.2 Building Design – Apartment Dwellings

Each apartment building is identical in terms of built form. Both buildings are 12 metres in height (four-storeys), with a building footprint of 1,030 square metres and a gross floor area of 4,140 square metres. Ingress and egress to the underground parking is located on the north side of Building B, connected to the new public road. The buildings include 130 underground parking stalls and 18 at-grade visitor stalls.



Figure 2: Perspective rendering of proposed apartment dwelling buildings.

### 1.2.3 Building Design – Townhouse Dwellings

The proposed development includes one townhouse buildings, which is comprised of six individual dwellings. The exterior dwellings (noted as “A” on the Site Plan) each have an area of 101 square metres. The exterior dwelling at the north-west side of the building has a lot area of 356 square metres and lot coverage of 28%, and the dwelling at the north-east side of the building has a lot area of 461 square metres and a lot coverage of 22%. The interior dwellings (noted as “B” on the Site Plan) are identical in built form. All interior dwellings have an area of 91 square metres, a lot area of 182 square metres, and a lot coverage of 50%. The height of the building is 12 metres.



Figure 3: Perspective rendering of the proposed townhouse dwellings.

**1.2.4 Plan of Subdivision**

A Draft Plan of Subdivision will be required to create the public street and the development blocks proposed to accommodate the dwellings. Each block proposed as part of the Plan of Subdivision is identified in Figure 8 below, with the area of each block as follows:

- / Block 1 – 9,865.1 square metres
- / Block 2 – 1,556.2 square metres
- / Block 3 – 7,299.5 square metres

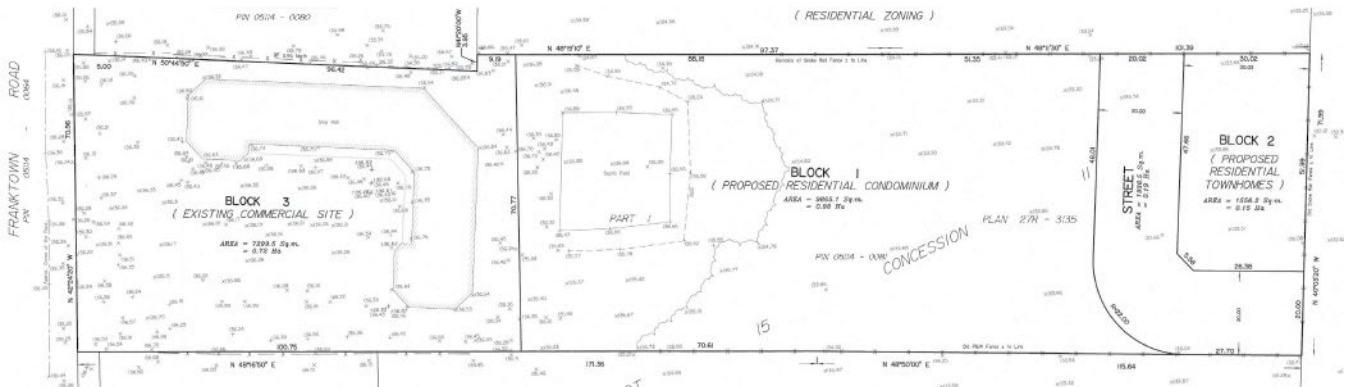


Figure 4: Proposed Draft Plan of Subdivision.

## 2.0 Physical Context

### 2.1 Site Description

The subject property, municipally known as 355 Franktown Road, is located in the south-east area of the Town of Carleton Place. The property is roughly 300 metres north of the Trans Canada Highway (Highway 7), in an area that is characterized to the east of the property by commercial uses and low-rise residential uses to the north and west. The total area of the subject property is 20,657.3 square metres and has a frontage on Franktown Road of 70.96 metres. The property is presently improved by a single-storey strip mall, which includes a convenience store, frozen foods store, and pet store. Existing development on the site is to be retained on a separate parcel to be created through the subdivision process. The proposed development represents an area of 13,357.8 square metres.



Figure 5: Aerial image of the subject property and surrounding area, identifying the area of new development in orange.



Figure 6: Street view of the subject property's existing development, looking north on Franktown Road.

## 2.2 Surrounding Area

The following land uses are located in the area surrounding the subject property:

### North

Immediately north of the subject property is vacant woodlands that is subject to an active development application for future residential units. Beyond the woodland area are large-format commercial uses including a department store, grocery store, and pharmacy.

### East

Immediately east of the subject property is primarily vacant land to the west of Highway 7. Development along Highway 7 is exclusively commercial uses. Included amongst these uses are several automobile dealerships, restaurants, and home improvement stores. Further east is a subdivision that is projected to continue growing to the east of Highway 7 and along Highway 15.

### South

Immediately south of the subject property is a subdivision that is primarily composed of single-detached residential units. Other uses in the area include an elementary school, a community centre and car dealership.

### West

To the west of the subject property is a residential subdivision that is comprised primarily of single-detached residential units. In addition to residential uses, the area includes McNeely Park and the Ottawa Valley Rail Trail.



Figure 7: Aerial view of the subject property and amenities found in the surrounding area.



## 2.3 Existing Conditions

### 2.3.1 Existing Development

The existing development on the subject property occupies the area fronting Franktown Road, to a depth of approximately 110 metres, and an area of 6,400 square metres. The area is composed of a single-storey commercial use strip mall. The associated parking lot is presently striped with 103 parking stalls. Access to the site is located at the southern frontage of the site, abutting 359 Franktown Road. Circulation throughout the site for large vehicles servicing the commercial units is undertaken by entering the site and driving to the rear of the building from the southern lane within the parking lot. Egress from the site is sought from the same location of ingress. As identified on the accompanying TCR, no trees are located in this section of the subject property.



Figure 8: 1 – View of existing development from entrance off Franktown Road, 2 – Rear drive aisle along north-west property line, 3 – View of commercial units, 4 – View of subject property looking north on Franktown Road.

### 2.3.2 Proposed Development Area

The area where the primary development is proposed is located at the rear of the property, with an area of approximately 13,358 square metres. The area is composed of grassland, which occupies the southern portion of the undeveloped lands, and woodlands which occupy the remainder of the site. The accompanying TCR provides a breakdown of the plant species found in the wooded area. Eastern White Cedar (*Thuja occidentalis*) makes up the majority of plant material in the wooded area. The accompanying ESA (Phase I and II) studies identify no significant environmental hazards on the site.



Figure 9: Views of the area of proposed development from the centre of the site (1) and from within the interior of the woodlands (2, 3, and 4).

## 2.4 Response to Existing Policy Consideration

### 2.4.1 Lanark County Sustainable Communities Official Plan

Lanark County is the upper-tier municipality for the Town of Carleton Place. Accordingly, Lanark County is the approval authority for the Carleton Place Official Plan and has jurisdiction over land division. The Town of Carleton Place Official Plan is required to be in conformity with the Lanark County Sustainable Communities Official Plan (SCOP), adopted in 2012.

The Sustainable Communities Official Plan (SCOP) recognizes two predominant settlement pattern types throughout the County:

- / Settlement Areas which can consist of fully serviced Towns and Villages and partially serviced or un-serviced Villages and Hamlets; and
- / Dispersed rural and waterfront uses which are or can be developed on existing lots of record or on lots created by plan of subdivision/condominium or by consent.

The subject property is located within a designated Settlement Area, specifically the Town of Carleton Place. The historical pattern of development in Lanark County consists of fully serviced (i.e. water and waste water infrastructure) Towns and Villages, partly serviced or un-serviced Villages and Hamlets, individual lots and rural subdivisions dispersed throughout the rural area and along hundreds of water bodies.

**The proposed development is located in the Town of Carleton Place on a lot on full municipal services.**



Figure 10: Schedule A – Land use, of the Lanark County Sustainable Communities Official Plan.

The settlement policies are intended to create a planning framework which will encourage and support diversified, mixed use Settlement Areas which have developed on the basis of full or partial municipal services or which are planned population centres to be developed on the basis of sustainable private services.

The proposed development meets the following Settlement Area policies, among others:

**2.3.1** Lanark County is home to many thriving Towns, Villages and Hamlets which provide a place to live, work and play. The following shall apply:

- / Settlement Areas are identified on Schedule A to this Plan. The limits of the Settlement Areas are in accordance with the limits established in local Official Plans.
- / Local Official Plans shall designate Settlement Areas and shall ensure that there is sufficient land area to accommodate a broad range of land uses to meet current needs and expected population growth over a maximum twenty-year timeframe.
- / Local Official Plans shall distinguish between fully serviced, partially serviced and un-serviced settlement areas and provide appropriate land use policies for each.
- / Local Official Plans shall promote intensification in existing built-up areas based on the type of servicing infrastructure.
- / Efficient development patterns will be encouraged in Settlement Areas to optimize the use of land, resources, infrastructure and public service facilities.
- / Local land use policies shall provide for mixed use development including residential, commercial, employment lands, parks and open space and institutional uses in areas designated as a settlement area in local Official Plans.

**The proposed development is located within the Settlement Area of the Town of Carleton Place and will contribute an influx of housing stock supporting expected population growth. Further the subject property is fully serviced and presents an efficient land pattern of development that will complement the future abutting development to the north and east of the subject property.**

- 2.6.1** The County of Lanark's objectives respecting development in Settlement Areas are as follows:
- / To ensure the provision of an adequate supply of residential land;
  - / To provide for a range and mix of low, medium and high-density housing types in accordance with servicing capacities;
  - / To provide for neighbourhood facilities and amenities which are appropriate to a residential living environment;
  - / To ensure the provision of roads and other municipal services necessary to the development of functional neighbourhood areas; and
  - / To provide for mixed use communities with appropriate commercial, institutional and employment uses.

**The proposed development will contribute to the adequate housing supply in Carleton Place through the form of a mix of housing types, both high-density and low-density. The subdivision will include the addition of a new public road connecting to an abutting subdivision, as well as two fire access lanes. The subject property is on full municipal services.**

- 2.6.2.3** Local Councils through the development of municipal zoning by-laws and the application, where required, of site plan control, shall regulate the development of settlement areas. The use of consent, subdivision and condominium control by the County in a cooperative process with local municipalities shall also apply.

**The proposed development requires an application for subdivision, please refer Section 4.1 of this Rationale for a further discussion on the appropriateness of the subject property for Subdivision. The development also includes a Development Permit application, addressing Development Permit Bylaw and site plan control considerations.**

- 2.6.2.4** The implementation of this Official Plan through local Official Plans, zoning regulations, subdivision and condominium control and site plan control shall consider the following criteria:
- / Permit and zone a range of residential housing types and sizes;
  - / Ensure development can proceed on appropriate and verified water, waste water, storm water and transportation services;
  - / Ensure adequate buffering of residential areas from incompatible non-residential uses through separation distance, landscaping or other appropriate means;
  - / Identify and zone an appropriate range of commercial, institutional and employment lands;
  - / Where applicable, identify and zone mixed-use areas in selected areas of the municipality;
  - / Permit increased housing densities through redevelopment of existing residential and non-residential buildings to meet locally established intensification targets where infrastructures permit;
  - / Permit accessory apartments in accordance with Section 16(3) of the Planning Act;
  - / Provide for open space and parkland and the protection of natural heritage features;
  - / Allow residential infill and redevelopment provided there is sufficient reserve capacity in water and waste water facilities;
  - / Regulate the physical character of infill or redevelopment projects to ensure their compatibility with established communities;

- / When reviewing applications for redevelopment or infill, consider the impact of the proposed development on the neighbourhood in terms of parking, traffic, open space, and proposed uses.

**The proposed development offers the redevelopment of underutilized land with multiple housing types on a fully serviced property that is compatible with the surrounding context and in line with the broader policies of the Lanark County Sustainable Communities Official Plan.**

**2.6.3** The creation of new lots in Settlement Areas shall generally occur through plans of subdivision or consent. The use of the Planning Act’s consent provisions may be appropriate under certain circumstances such as infill development and where a limited number of new lots are proposed to be created. The criteria identified for land division by consent in local Official Plans shall apply.

**Please refer to Section 4.1 of this Rationale for the appropriateness of a subdivision using the Planning Act’s consent provisions.**

**2.4.2 Town of Carleton Place Official Plan**

All development within the Town must be in conformity with the Town of Carleton Place Official Plan, approved in 2014. The Council’s Vision for Carleton Place, as defined in Section 1.2 of the Official Plan, is:

“The Town of Carleton Place is committed to maintaining and celebrating its heritage through balanced and sustainable growth which will support a sense of place respectful of our unique historical, cultural and natural heritage where citizens can enjoy an unparalleled quality of life.”



Figure 11: Schedule A – Town of Carleton Place Official Plan.

Section 1.3 sets out the Guiding Principles that apply to the proposed development. Relevant principles include:

- / Principle 2 – We will ensure that growth and development occurs through sustainable and economically viable land use development patterns which will include a broad range of uses and a balanced mix of appropriate residential densities;
- / Principle 5 – We will ensure appropriate development which will not pose a danger to public safety or health or result in negative property or environmental impacts; and
- / Principle 6 – We will ensure that effective infrastructure services will be provided by the appropriate level of government or the private sector in a cost-efficient manner which recognizes development priorities and which ensures the protection of our environment.

Section 2.0 provides an overview of the Community Design Framework, emphasizing the “vital importance that on-going changes to the built form be undertaken through high quality developments that are integrated with the surrounding community. It is these policies that are applied to all new proposed development and forms the bases for the regulatory framework for the Town’s Development Permit By-law, which is discussed in detail in Section 4.5 of this Planning Rationale.

The most relevant policies to the proposed development under Section 2.3 (General Design Policies) are as follows:

**2.3.1 Proposed developments shall enhance the image of the Town of Carleton Place by complementing and contributing to:**

- / **the character of the area;**
- / **local landmarks;**
- / **the consistency and continuity of the area with its surroundings;**
- / **the edges of the area; and**
- / **linkages within, to and from the area.**

**The proposed development proposes residential uses within an area characterized for residential growth and provides connections to an adjacent subdivision, promoting linkages and integration within the local context.**

**2.3.6** The design of new development shall:

- / Be complementary to adjacent development in terms of its overall massing, orientation and setback;
- / provide links with pedestrian, cycling and road networks;
- / enhance orientation and integrate newly developing areas of the Town of Carleton Place;
- / and
- / maintain and enhance valued cultural and heritage resources and natural features and functions.

**The proposed development includes buildings which are appropriately sized and oriented, including street fronting townhouses, for a growing residential community in proximity to services and amenities within the adjacent Highway District. Further, the proposal sensitively accounts for and integrates with adjacent planned development through a new public road.**

**2.3.7** Development or redevelopment design shall strive to achieve the following:

- / provide a development pattern that supports a range of uses;
- / provide transportation connections, including pedestrian and cycling connections to adjacent areas; and

/ maintain and enhance valued historic development patterns and resources.

**The proposed layout preserves the existing commercial spaces along Franktown Road while making efficient use of the remainder lands for new residential uses. These include a new public road for connections to other surrounding areas of growth.**

The subject property abuts the Mississippi District Thoroughfare, which is recognized as a collection of specific roads in the Town of Carleton Place that are integral to future development. The thoroughfare has the ability to link newer areas of development and major transportation routes to the Town's core area.

Section 3.2.3.1 outlines the policies pertaining to the Mississippi District Thoroughfare and how it pertains to new development. Relevant policies that apply to the proposed development area include:

**3.2.3.1.2** In maintaining Mississippi District Thoroughfares Council may embark on street beautification programs and shoreline improvement programs and shall ensure that all public works carried out along these Thoroughfares contribute to their preservation and enhancement. Council shall also ensure that any private development initiatives along Mississippi District Thoroughfares respect and are consistent with the local character and environmental conditions.

**The portion of the subject property that fronts the Mississippi District Thoroughfare is well landscaped and presents an appealing entrance to and from the subject property.**

**3.2.3.1.3** Permitted uses on lands fronting on a Mississippi District Thoroughfare shall generally be in accordance with the land use designation shown on Schedule A and the related policies in this Plan.

**The subject property is designated Residential Use on Schedule A of the Town of Carleton Place Official Plan. The proposed development seeks to add a total of 102 residential dwellings to the subject property in the form of two apartment buildings and a townhome building.**

The subject property is located within the Residential District designation, as shown on Schedule A of the Official Plan. Land Use Policies for this are set out in Section 3.5 of the Official Plan. The intent of the Residential District is to promote sustainable, efficient and diverse residential neighbourhoods and provide a diverse range of housing types and densities.

The proposed development meets the following Residential District policies articulated in Section 3.5.3, among others:

**3.5.3.1** Where land is designated Residential District on Schedule A to this Plan, a range of residential dwelling types and densities shall be permitted, including single detached, semi-detached, duplex dwellings, triplex dwellings, townhouse dwellings and apartment dwellings.

**The proposed development looks to develop 96 apartment dwelling units and 6 townhouse dwelling units. Both housing types and associated densities are permitted within the Residential District designation.**

Section 3.5.4 outlines Density Provisions which are intended to ensure that new development will include a mix of residential densities to address a full range of housing options.

The proposed development meets the following Density Provisions policies articulated in Section 3.5.4, among others:

**3.5.4.2** Notwithstanding Section 3.5.4.1, where development is proposed on infill sites or sites which are the result of lot consolidations, and which infill sites or consolidated sites have areas of 3 hectares or less, residential density may be increased. In such cases density will be controlled through the regulatory framework of the Development Permit By-law

**Please see Section 4.5 of this Rationale for the compliance of the proposed development to the Development Permit By-law.**

**3.5.4.3** In areas subject to Section 3.5.4.2 above, the requirement for a mix of dwelling types as required in Section 3.5.4.6 shall not apply.

**Despite the above, the proposed development includes a mix of both apartment dwelling units and townhouse dwelling units.**

**3.5.4.4** The following residential density classifications shall apply:

- / High density: includes apartments in excess of 35 units per net hectare (14 units per net acre).
- / Medium density: includes town or row houses and apartments in a range of greater than 2 units per net hectare (9 units per net acre) up to a maximum of 35 units per net hectare (14 units per net acre).

**The proposed development has an overall density of 90 units per hectare. Block 1 has density of 98 units per hectare (96 units on 0.98 hectares of land) and Block 2 has a density of 40 units per hectare (6 units on 0.15 hectares of land).**

**3.5.4.5** New medium or high-density residential development shall be subject to the following policies:

- / The proposed design of the residential development is compatible in scale with the character of surrounding uses;
- / The site is physically suited to accommodate the proposed development;
- / The proposed site can be serviced with adequate water and waste water services;
- / The property shall have appropriate access to an arterial or collector road maintained to a municipal standard with capacity to accommodate traffic generated from the site;
- / Sufficient off-street parking facilities is provided in accordance with the standards set out in the Development Permit By-law; and
- / The development can take place in accordance with the policies of Section 2.0.

**The proposed development qualifies as a high-density residential development (>35 units / hectare). The development remains compatible with the scale of development for the surrounding area and broadly meets the Development Permit Bylaws. The property fronts an arterial road and provides sufficient parking per municipal requirements. Further, the development is fully compliant with the policies found in Section 2.0 of the Town of Carleton Place Official Plan.**

**3.5.4.6** New residential development shall include a mix of residential densities. Residential development which does not provide a diversity of dwelling types shall be discouraged.

**The proposed development includes housing units in the form of apartment dwellings and townhouse dwellings.**

**3.5.4.7** Development shall be integrated with surrounding development, through connected street networks, appropriate transition of housing types and densities and through supporting infrastructure including recreational pathways and parks.

**The proposed development is projected to be part of a broader development of the vacant area along Franktown Road. Coordination efforts have been made between developers to align street networks and housing types between subdivisions.**



**2.4.3 Town of Carleton Place Development Permit Bylaw**

In 2015, the Town of Carleton Place passed Development Permit By-law 15-2015 (the By-law). The development permit system consolidates the functions of several separate development approvals processes into a single process, including zoning, site plan control and minor variance approvals. The Development Permit By-law must be in conformity with the in-force local Official Plan.



Figure 12: Development Permit Schedule A - Town of Carleton Place.

Town Staff have confirmed that the proposed development is subject to a Subdivision/Condominium Application, subject to approval by Planning Committee, due to the scale and intended uses of the proposed development, as well as the requirement of both a public and private road. Approvals may be conditional on meeting specific conditions such as dedication of lands for highway widening, drainage easements and parkland conveyances.

The Development Permit By-law is structured in multiple sections. Section 3.0 contains general provisions that apply to all lands within the town; Section 6.0 contains provisions pertaining to the Residential District; and Section 14.0 contains Design Criteria. All are relevant to the proposed development.

The intent of the Residential District is to promote compatible residential development within neighbourhoods. Infill development must consider existing built forms and fit into the surrounding landscape with minimal impact. The tables below summarize development standards required under Section 6.3 and Section 3.0 of the By-law for Townhouse Dwellings and Apartment Dwellings.

**Table 1 – Townhome Dwelling Development Standards**

Site Provisions	Requirements	Proposed	Compliance
Lot Area Minimum	No Minimum	20,657.3 square metres	✓
Lot Coverage (Max.)	60%	<60%	✓

<b>Lot Frontage (Min.)</b>	5.5 metres	6.1 metres	✓
<b>Front Yard Build Within Area</b>	4.5 m (Min.), 7.5 m (Max.)	6.0 metres	✓
<b>Exterior Side Yard Build Within</b>	4.5 m (Min.), 7.5 m (Max.)	8.5 metres	✗
<b>Interior Side Yard (minimum)</b>	1.5 metres	5.3 metres	✓
<b>Rear Yard Depth (minimum)</b>	6.5 metres	7.5 metres	✓
<b>Usable Landscaped Open Space in the rear yard (minimum)</b>	30.0 square metres	45.75 square metres	✓
<b>Building Height (maximum)</b>	11.0 metres	6.0 metres	✓
<b>Minimum Dwelling Unit Area</b>	83.1 square metres	91.0 square metres	✓
<b>No Encroachment Area from front or exterior side lot line</b>	2.5 metres	5.5 metres	✓
<b>Parking Spaces</b>	2 spaces per dwelling	2 spaces	✓

**Table 2 – Apartment Dwelling Development Standards**

<b>Site Provisions</b>	<b>Requirements</b>	<b>Proposed</b>	<b>Compliance</b>
<b>Lot Area Minimum</b>	No Minimum	20,657.3 square metres	✓
<b>Lot Coverage (Max.)</b>	60%	<60%	✓
<b>Lot Frontage (Min.)</b>	35.0 metres	39.2 metres	✓
<b>Front Yard Build Within Area</b>	4.5 m (Min.), 7.5 m (Max.)	10.6 metres	✗
<b>Exterior Side Yard Build Within</b>	4.5 m (Min.), 7.5 m (Max.)	No Exterior Side Yard	✓
<b>Interior Side Yard (minimum)</b>	1.5 metres	7.5 metres	✓
<b>Rear Yard Depth (minimum)</b>	6.5 metres	11.7 metres	✓
<b>Usable Landscaped Open Space in the rear yard (minimum)</b>	20%	>20%	✓
<b>Building Height (maximum)</b>	14.0 metres	12 metres	✓
<b>No Encroachment Area from front or exterior side lot line</b>	2.5 metres	5.5 metres	✓
<b>Parking Spaces</b>	1.5 spaces per dwelling (1.25 + 0.25 visitor)	148 spaces	✓

As indicated in the development bylaw tables above, the proposed development is broadly compliant with the development standards for both townhouse dwellings and apartment dwellings. It should be noted that the Development Permit System is designed to incorporate flexibility and to empower Staff and Council to reasonably vary the by-law to better respond to specific applications. As set out in Section 3.44 – Variations, “Council may vary the

standards, provisions and requirements of the Development Permit bylaw [...] provided that the proposal is consistent with and complies with both the Official Plan of the Town of Carleton Place and the Provincial Policy Statement 2014.”

Therefore, the following variations to the Development Permit requirements are requested, if deemed necessary:

- / To confirm and permit the proposed townhouse to be built 8.5 metres from the exterior property lot line, exceeding the permitted build within area of between 4.5 metres and 7.5 metres; and
- / To confirm and permit the proposed apartment to be built 10.6 metres from the front yard property lot line, exceeding the permitted build within area of between 4.5 metres and 7.5 metres.

#### **2.4.4 Rationale for Requested Variances from Development Standards**

##### **Increase Exterior Side Yard Build Within Area (Townhouse Dwellings)**

- / Whereas townhouse dwellings are only permitted to be between 4.5 metres and 7.5 metres from an exterior side yard, the proposed variance would increase the permitted exterior side yard distance to 8.5 metres for townhouse dwellings.
  - The proposed increase to the townhouse dwelling exterior side yard setback is slight in nature (1 metre) and has a limited effect on the surrounding proposed development. The proposed townhouses were positioned so as to better align with the proposed future development of townhouses on the abutting property to the north. By positioning the single row of townhouses to be compliant with the interior side yard build within area, the result was to be deficient on the exterior side yard build within area. Given the location of the townhouses within the broader subdivision and future development of the area, the increase of one metre from the exterior property line presents no meaningful difference to the urban form of the area. Therefore, the increase in the exterior yard build within area to 8.5 metres is a reasonable variation to the existing Development Permit Bylaw.

##### **Increase Front Yard Build Within Area (Apartment Dwellings)**

- / Whereas apartment dwellings are only permitted to be between 4.5 metres and 7.5 metres from the front yard property lot line, the proposed variance would increase the permitted front yard distance to 10.6 metres for apartment dwellings.
  - The proposed increase to the apartment dwelling front yard setback has limited effect on surrounding development. It is acknowledged that “build between” setbacks are designed to regulate development with a similar setback from property lot lines. However, the proposed apartments do not abut any proposed development, which limits any adverse effects on regulating setbacks among developments along a shared street. Therefore, the increase in the front yard build within area to 10.6 metres is a reasonable variation to the existing Development Permit Bylaw.

## 3.0 Massing and Scale

PREPARED BY PMA ARCHITECTES



**FRANKTOWN DEVELOPMENT  
PRESENTATION FOR THE MUNICIPALITY OF CARLETON PLACE**

**OCTOBER 17, 2022**

**PROPOSITION**

**1.0**

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SITE PLAN  
BUILDING MASSING  
PLANS AND ELEVATIONS

**PERSPECTIVES**

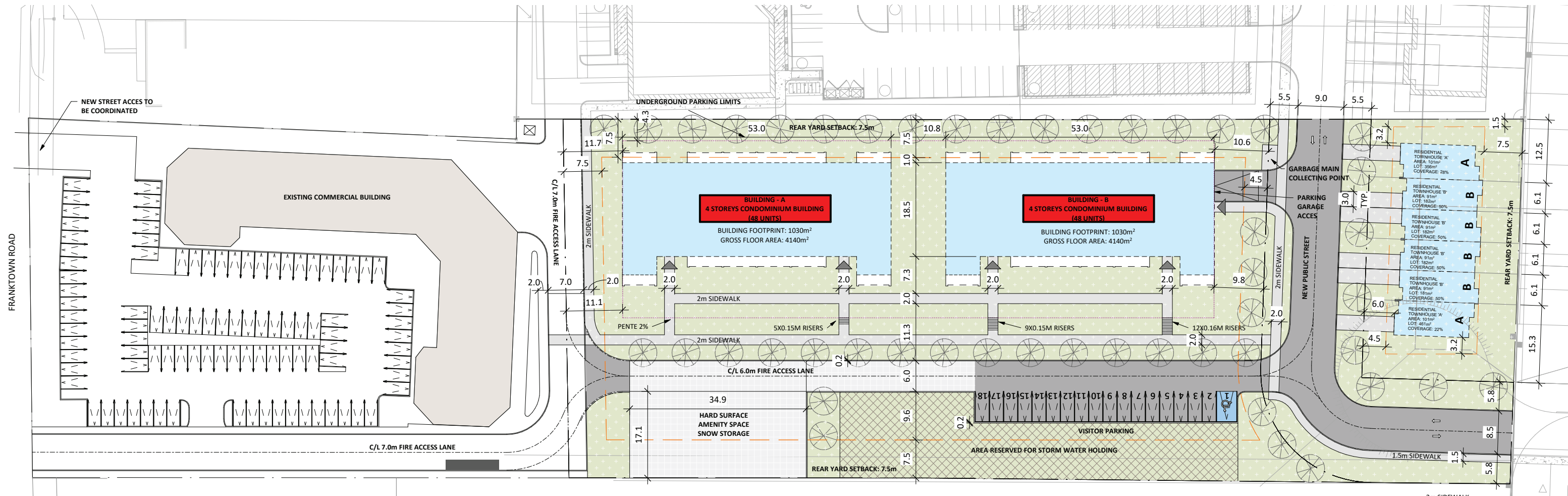
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# 1.0 PROPOSITION

# 1.1 SITE PLAN



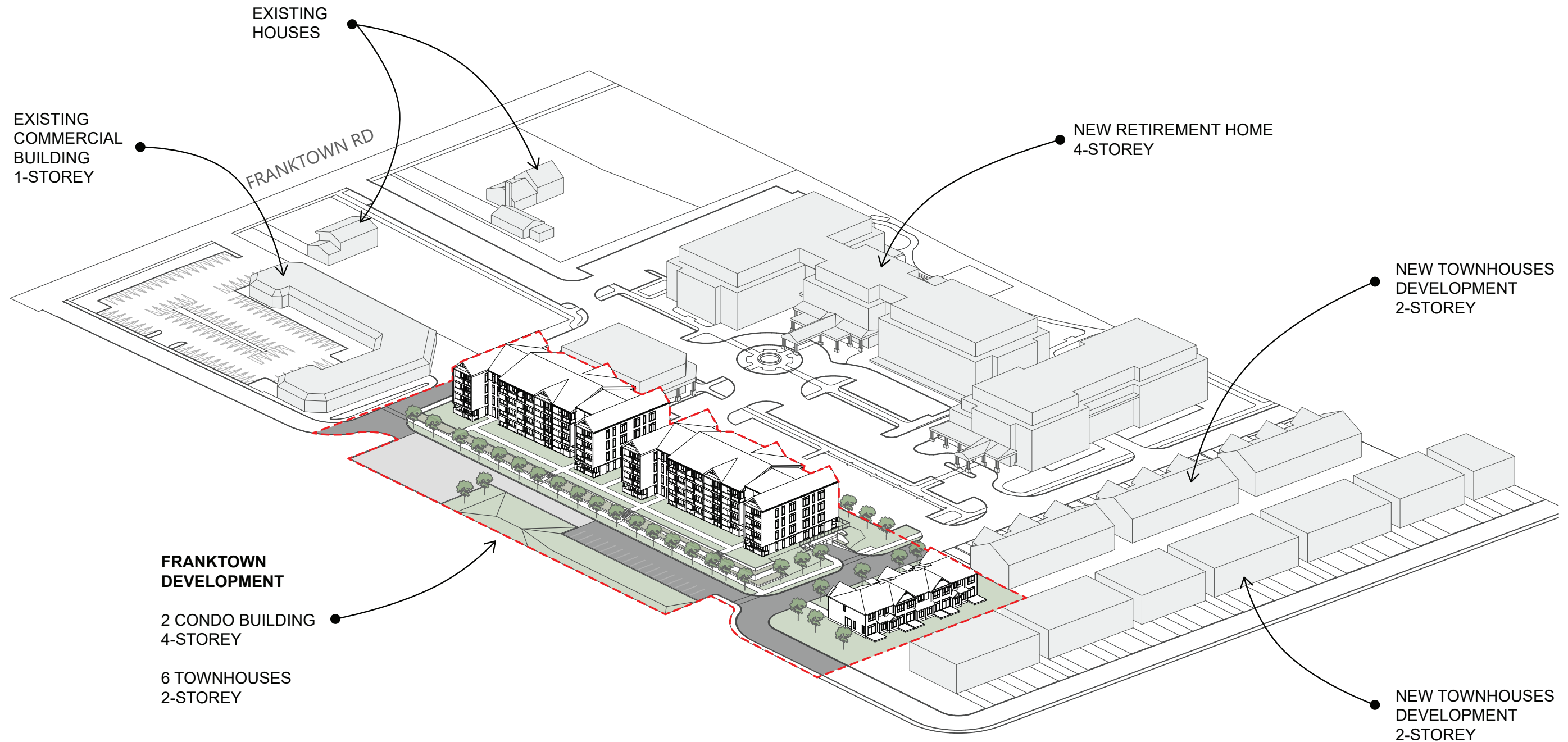


**LEGEND :**

- - - - - LOT LINE
- - - - - SETBACK
- - - - - UNDERGROUND PARKING LIMIT
- ASPHALT
- HARD SURFACE / SPACE FOR SNOW STORAGE
- AREA FOR STORM WATER HOLDING
- GRASS
- TREES



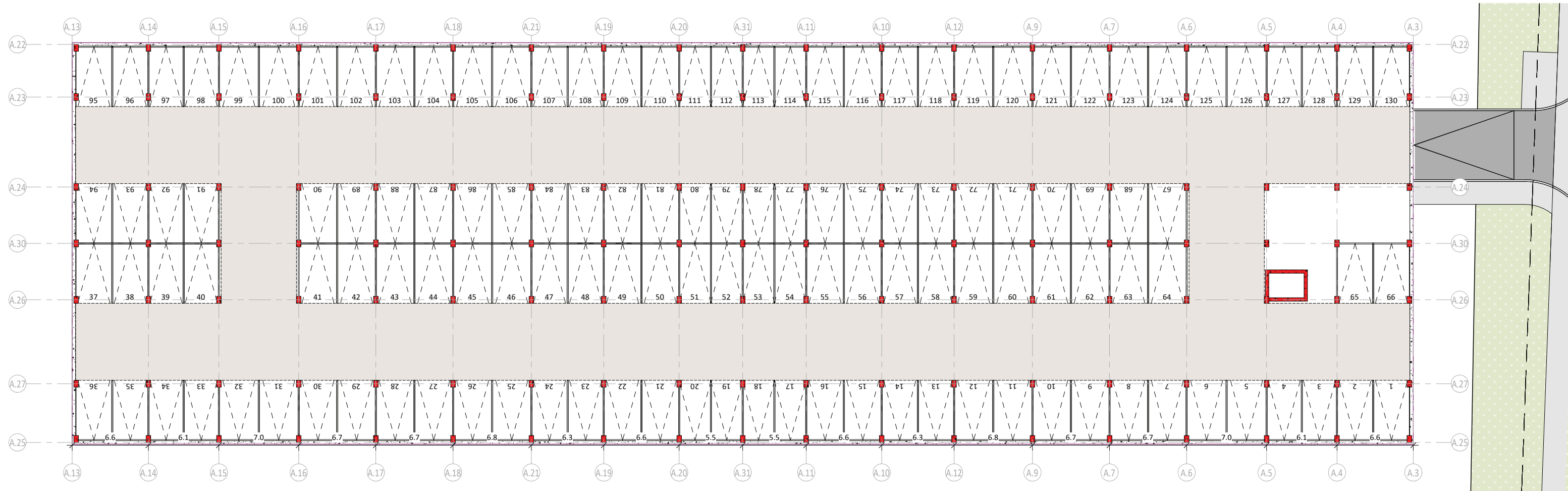
# 1.2 BUILDING MASSING





# 1.3 PLANS AND ELEVATIONS

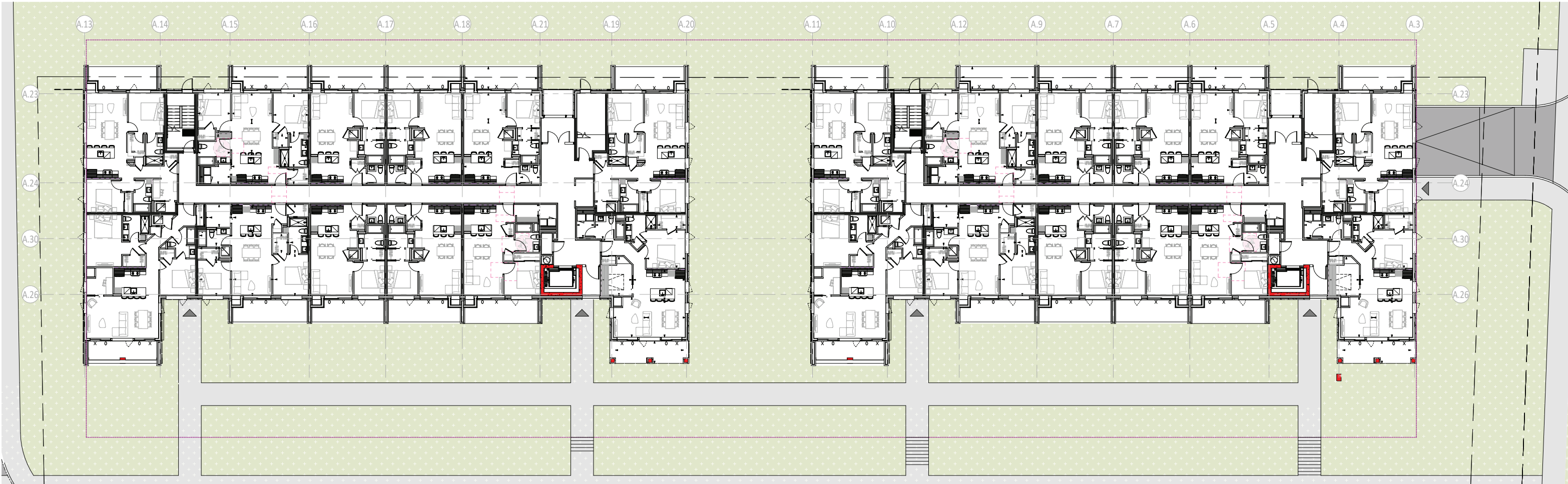
**4 STOREYS CONDOMINIUM LEVEL S1**



Number of underground parking spaces : 130

Total parking spaces : 148



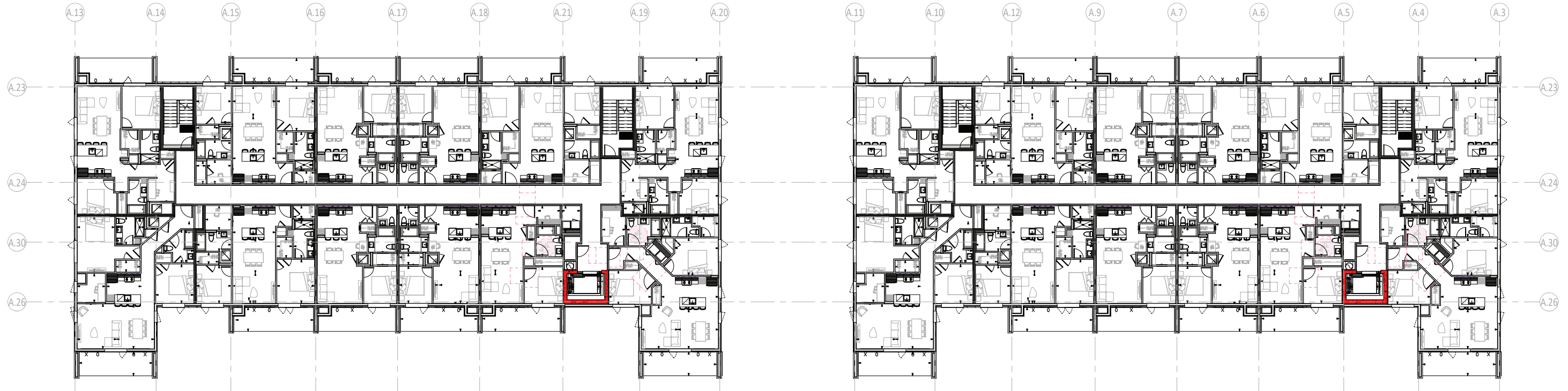


Number of units on 1st floor: 12 units per building

Total units per building : 48 units



4 STOREYS CONDOMINIUM LEVEL 2 - 3 - 4



Number of units on 1st floor: 12 units per building

Total units per building : 48 units







**FRONT ELEVATION  
SOUTH-EAST**





**BACK ELEVATION  
NORTH-WEST**





FRONT ELEVATION  
SOUTH-EAST





**LEFT ELEVATION  
SOUTH-WEST**



MASONRY - PERMACON LAFITT STONE  
COLOR RANGE - MARGAUX BEIGE



VERTICAL BOARD AND BATTEN  
SIDING JAMESHARDIE  
SMOOTH COLOR - TIMBER BARK



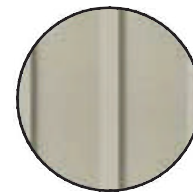
HORIZONTAL LAP SIDING JAMESHARDIE  
CEDARMILL COLOR - TIMBER BARK



**BACK ELEVATION  
NORTH-WEST**



MASONRY - PERMACON LAFITT STONE  
COLOR RANGE - MARGAUX BEIGE



VERTICAL BOARD AND BATTEN  
SIDING JAMESHARDIE  
SMOOTH COLOR -  
TIMBER BARK / COBBLE STONE



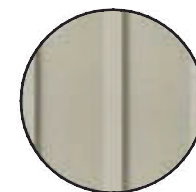
HORIZONTAL LAP SIDING JAMESHARDIE  
CEDARMILL COLOR - TIMBER BARK



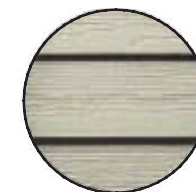
**BACK ELEVATION  
NORTH-WEST**



MASONRY - PERMACON LAFITT STONE  
COLOR RANGE - MARGAUX BEIGE

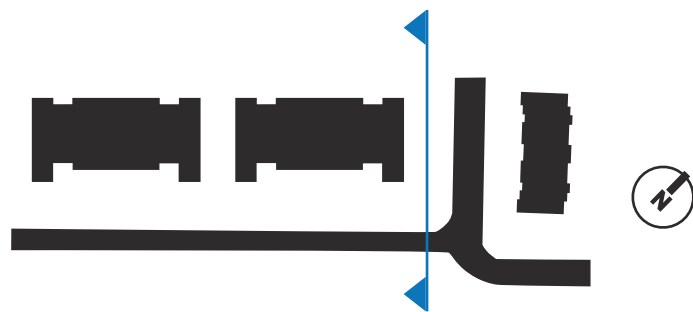


VERTICAL BOARD AND BATTEN  
SIDING JAMESHARDIE  
SMOOTH COLOR - COBBLE STONE

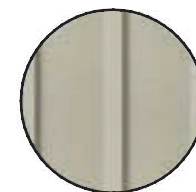


HORIZONTAL LAP SIDING JAMESHARDIE  
CEDARMILL COLOR - COBBLE STONE

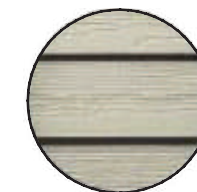
**RIGHT ELEVATION  
NORTH-EAST**



MASONRY - PERMACON LAFITT STONE  
COLOR RANGE - MARGAUX BEIGE



VERTICAL BOARD AND BATTEN  
SIDING JAMESHARDIE  
SMOOTH COLOR - COBBLE STONE



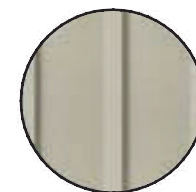
HORIZONTAL LAP SIDING JAMESHARDIE  
CEDARMILL COLOR - COBBLE STONE



**FRONT ELEVATION  
SOUTH-EAST**



MASONRY - PERMACON LAFITT STONE  
COLOR RANGE - MARGAUX BEIGE



VERTICAL BOARD AND BATTEN  
SIDING JAMESHARDIE  
SMOOTH COLOR - COBBLE STONE



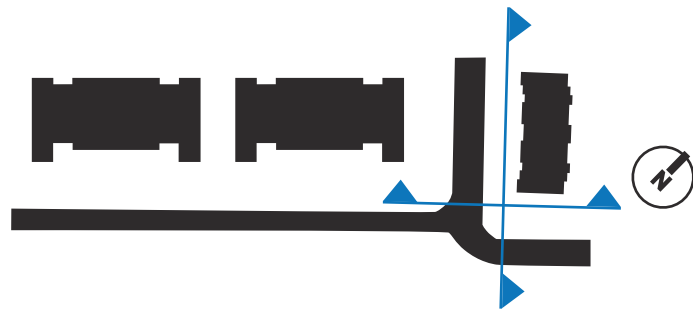
HORIZONTAL LAP SIDING JAMESHARDIE  
CEDARMILL COLOR - COBBLE STONE





**FRONT ELEVATION  
SOUTH-WEST**

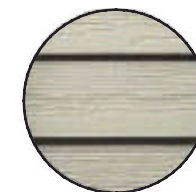
**RIGHT ELEVATION  
SOUTH-EAST**



MASONRY - PERMACON LAFITT STONE  
COLOR RANGE - MARGAUX BEIGE



VERTICAL BOARD AND BATTEN  
SIDING JAMESHARDIE  
SMOOTH COLOR - COBBLE STONE



HORIZONTAL LAP SIDING JAMESHARDIE  
CEDARMILL COLOR - COBBLE STONE

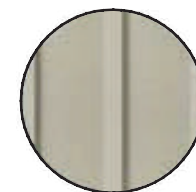


**BACK ELEVATION  
NORTH-EAST**

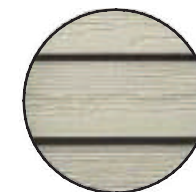
**LEFT ELEVATION  
NORTH-WEST**



MASONRY - PERMACON LAFITT STONE  
COLOR RANGE - MARGAUX BEIGE

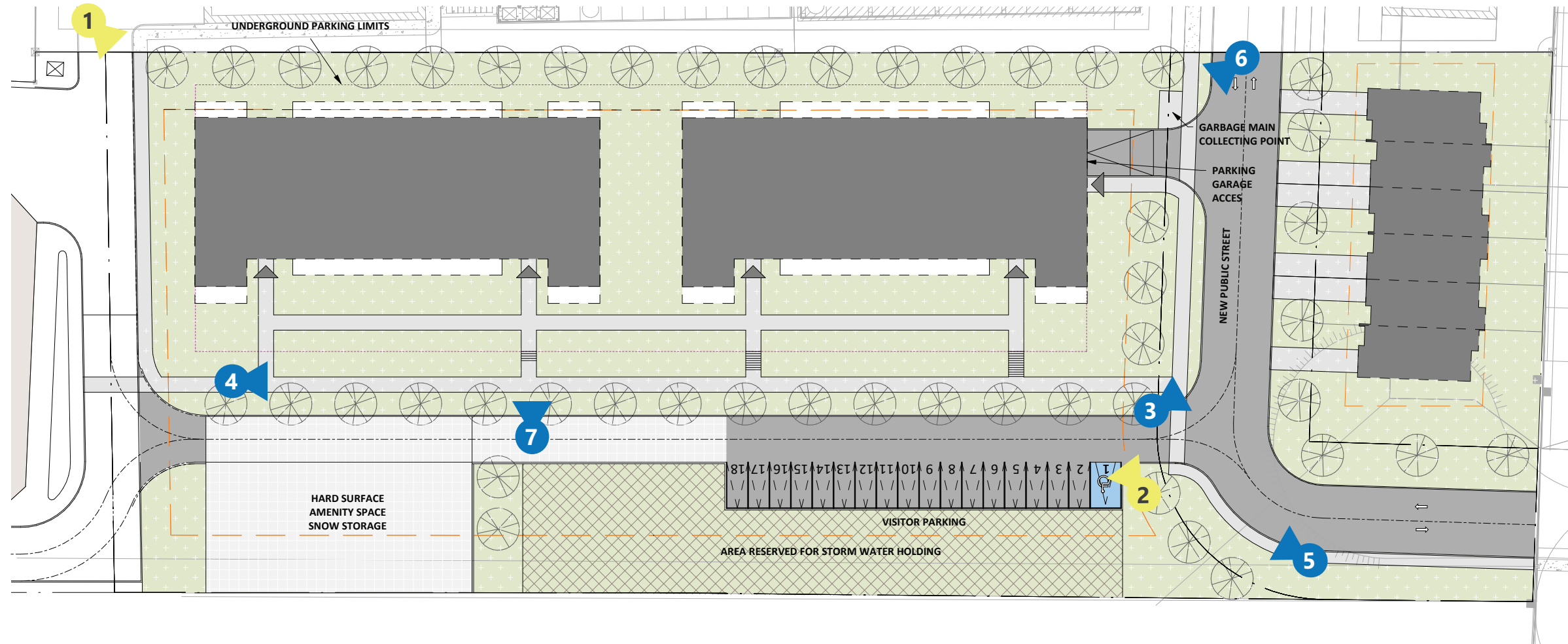


VERTICAL BOARD AND BATTEN  
SIDING JAMESHARDIE  
SMOOTH COLOR - COBBLE STONE



HORIZONTAL LAP SIDING JAMESHARDIE  
CEDARMILL COLOR - COBBLE STONE

# 2.0 PERSPECTIVES



 BIRD'S EYE VIEWS

 PEDESTRIAN VIEWS



**VIEW 1**  
GENERAL OVERVIEW  
OF THE PROJET





**VIEW 2**  
MAIN EXTERIOR  
ENTRANCE TO  
CONDOMINIUM B





**VIEW 3**  
TOWNHOUSES  
FRONT VIEW





**VIEW 4**  
PEDESTRIAN VIEW  
FROM THE FRONT  
SIDEWALK







**VIEW 5**  
VIEW FROM THE  
SIDEWALK OF THE  
MULTIPLE DWEL-  
LING UNITS





**VIEW 6**  
UNDERGROUND  
PARKING ENTRANCE





**VIEW 7**  
VIEW OF BUILDING  
A MAIN ENTRANCE

